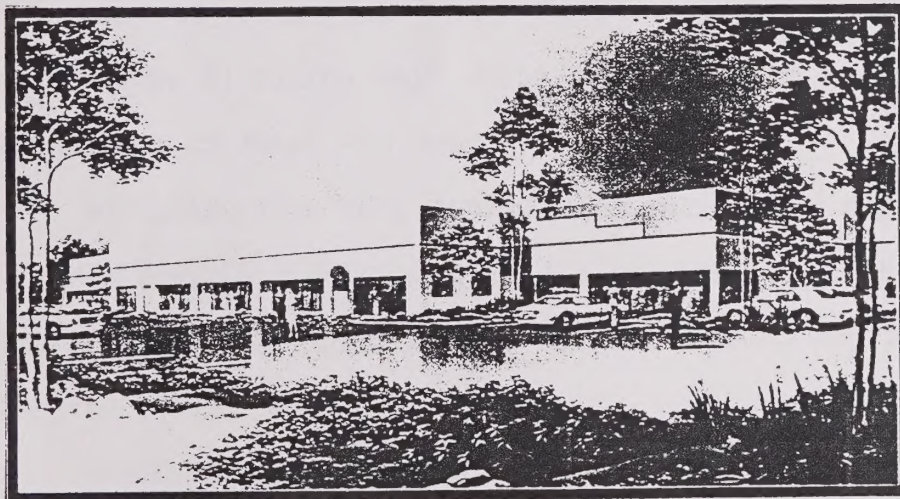


SOUTHWEST DINUBA SPECIFIC PLAN



City of Dinuba

Community Development Department

July 1992

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CITY OF DINUBA
ORDINANCE NO. 92-13

AN ORDINANCE OF THE CITY OF DINUBA
ADOPTING THE SOUTHWEST DINUBA
SPECIFIC PLAN

WHEREAS, Government Code Section 65450 through 65457 provide
for the development, adoption and implementation of Specific
Plans as a means of implementing the General Plan; and

WHEREAS, the City of Dinuba adopted an updated General Plan
in 1988; and

WHEREAS, the Dinuba City Council has caused to be prepared
the Southwest Dinuba Specific Plan which amends and implements
the General Plan for that portion of the community generally
bounded by El Monte Way, Alta Avenue, Kamm Avenue and the future
alignment of Road 72; and

WHEREAS, the City Council established a public review period
for the Specific Plan and held a notice public hearing to receive
comments on the Specific Plan and associated amendments to the
General Plan; and

WHEREAS, the Dinuba Planning Commission has reviewed the
Specific Plan and recommended approval with modifications and the
City Council has considered the Planning Commission's
recommendations; and

WHEREAS, the City Council, acting as the Lead Agency, has
certified as complete and adequate a Final Environmental Impact

Report for the project in accordance with the California Environmental Quality Act (CEQA), the CEQA Guidelines, and local requirements.

NOW, THEREFORE, BE IT RESOLVED, the Dinuba City Council hereby takes the following actions:

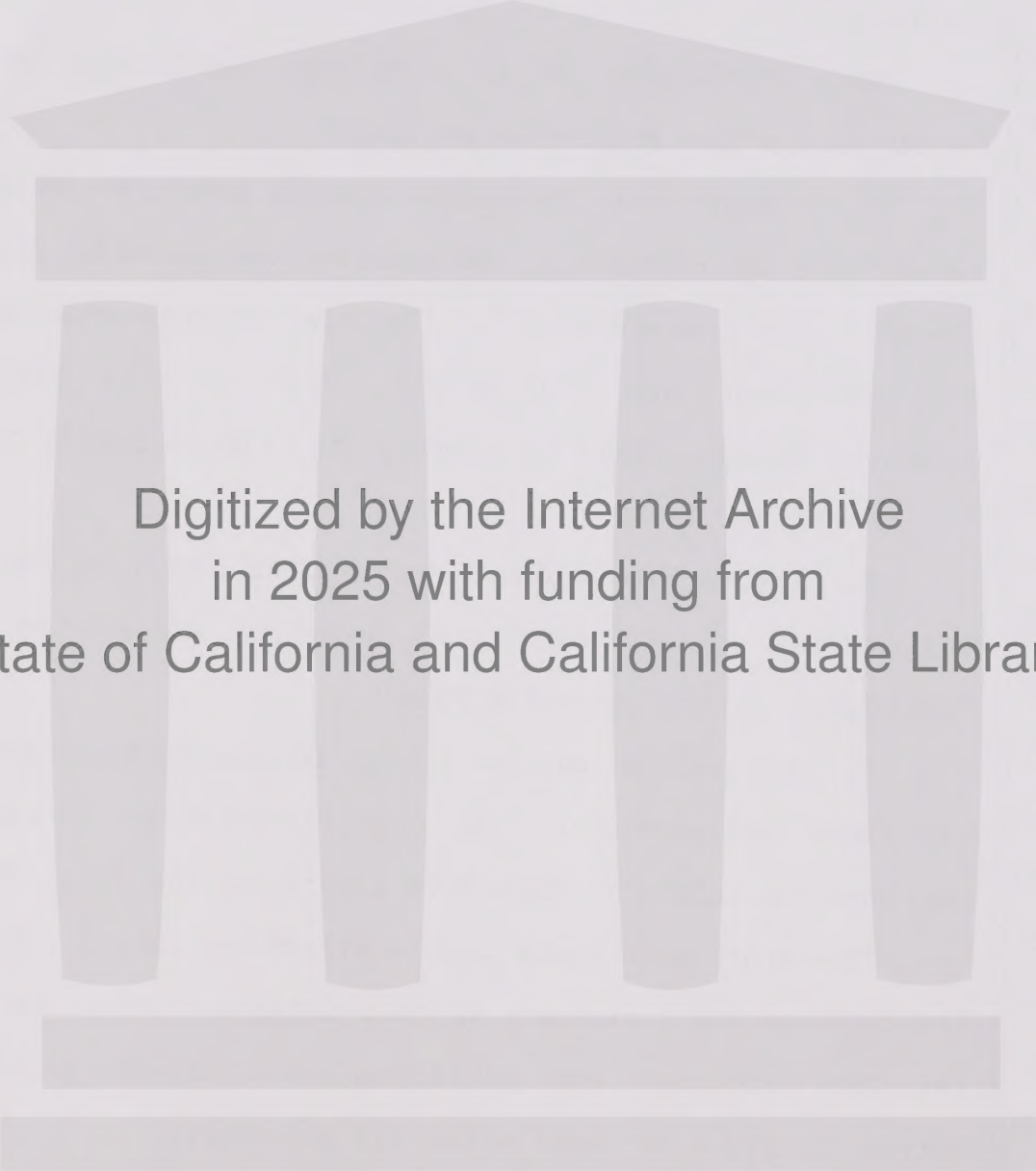
1. Adopts the Southwest Dinuba Specific Plan, attached hereto and by reference incorporated herein with the following change: The designation for the area located approximately 375 feet south of west Sierra Way, west of Alta Avenue (Dinuba Buddhist Church) shall be changed from single family residential; R-1 to Office; P-O.

2. Amends the General Plan Map as shown on Exhibit "A" by delineating the specific plan boundary and adding the title, "Southwest Dinuba Specific Plan".

3. Amends the Dinuba Zoning Ordinance by adding the zone districts delineated on the Specific Plan to those properties currently within the corporate limits of the City of Dinuba.

This Ordinance shall become effective thirty (30) days after its passage and a summary of this ordinance shall be prepared by the City Attorney and published once in the Dinuba Sentinel within thirty (30) days after its passage.

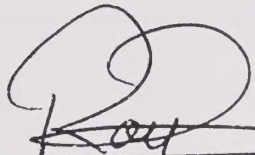
This Ordinance was introduced and read on this 28th day of July, 1992, and passed on the 11th day of August, 1992, by the following vote:



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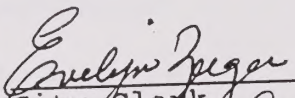
<https://archive.org/details/C124913144>

AYES: de la Montanya, Lankford, Millard, Westmoreland,
Fudge
NOES: None
ABSENT: None
ABSTAIN: None



MAYOR, CITY OF DINUBA

ATTEST:



City Clerk Deputy

ACKNOWLEDGEMENTS

Dinuba City Council

Ray Fudge
Mayor

Raymond K. Millard
Vice-Mayor

Barbra Lankford
Council Member

J.B. Westmorland
Council Member

John de la Montanya
Council Member

J. Edward Todd
City Manager

Dinuba City Planning Commission

Edward Koobation
Chairman

Les Felland
Vice Chairman

Paulie Romero
Commissioner

Nancy Harris
Commissioner

Steven Sunderland
Commissioner

Nina Northcutt
Commissioner

Bill Vuich
Commissioner

City Staff

Milton A. Tromborg, Community Development Director
Ed Paz, Principal Planner
Mike Henley, Parks & Recreation Director
Dan Meinert, Public Works Director

Consultant

Land Use Associates
1285 West Shaw Avenue, Suite 104
Fresno, California 93711

PLAN SUMMARY

The Southwest Dinuba Specific Plan addresses land use, circulation and public facilities planning for approximately 626 acres of land in the southwest area of the community. The plan is designed to systematically implement the objectives and policies and amend the Dinuba General Plan for the area it encompasses and is adopted in the same manner and has the same authority as the general plan. All actions related to development of incorporated land within the area encompassed by the plan must be consistent with the plan.

The major directives of the plan for development within the plan area are:

- Land within the plan area is designated for a combination of industrial, commercial, office, and residential uses. Zoning is also proposed to implement the land use districts.
- All development within the plan area will occur within the City. The plan contains provisions for annexing land and coordinating with Tulare County to promote this objective.
- The City will expedite the processing of applications for development within the plan area. The City will also assist industries in locating suitable sites for development within the plan area and in identifying financing for development-related activities.
- The major street system for the plan area will consist of El Monte, Alta, and Kamm planned four-lane arterial streets, and West Sierra Way and Industrial Way, both planned as four-lane collector streets. Industrial Way will be extended South of El Monte as an expansion of Alice Avenue. Road 72 will form the western boundary of the plan area and will be constructed as development occurs. Kamm Avenue forms the southern boundary of the project area.
- All new development within the plan area will use City water and sanitary sewer systems, and must be compatible with these systems.

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CHAPTER 1

INTRODUCTION

The Southwest Dinuba Specific Plan is a component of the Dinuba General Plan. The general plan was adopted in 1988 and provides overall guidance for residential, commercial and industrial growth and development within the Dinuba Planning Area. This specific plan augments and amends the general plan by providing more detailed planning of the southeast portion of the planning area. The plan area encompasses approximately 626 acres of land located at the southwest corner of the City, and is the primary area available to the City for industrial growth and development.

The specific goals of the City in authorizing the preparation of this plan are as follows:

- To demonstrate to existing and prospective industries the City's commitment to development within the plan area.
- To establish detailed land use, circulation and public facilities planning.
- To foster coordination and cooperation with property owners, industries, and businesses within the plan area.
- To serve as a support document for LAFCO Sphere of Influence amendments and annexations.

This plan is divided into seven chapters. Chapter 1 explains the purpose and scope of the plan. Chapter 2 describes the location of the plan area and information on its community setting. In Chapter 3, the objectives, policies and implementation measures for land use and development within the plan area are described. Chapters 4 and 5 describe the objectives, policies and implementation measures for circulation and public facilities, respectively. Chapter 6 contains policies on environmental resources and design. Chapter 7 discusses implementation.

CHAPTER 2

PLAN AREA DESCRIPTION

2.1 Location

The location of the City of Dinuba in the County of Tulare is shown on Figure 1. Dinuba is situated in the northwestern part of the county, about 12 miles east of Freeway 99, 14 miles north of Visalia, and 32 miles southeast of the Fresno/Clovis Metropolitan Area.

The boundaries of the area encompassed by this plan and the location of the plan area within the City of Dinuba are shown on Figure 2. The 626-acre plan area encompasses the land located between El Monte Way on the north; Alta Avenue on the east; Kamm Avenue on the south; and the future alignment of Road 72 on the west. West Sierra Way, an existing collector street, bisects the plan area in an east/west direction.

2.2 Land Ownership

The project area is made up of 52 parcels. The major property owner is the Surabian family which owns approximately 260 acres north of West Sierra Way, or 40% of the plan area.

2.3 Community Profile

Prior to irrigation, the San Joaquin Valley was once a vast, arid grassland. The valley is now a rich agricultural center producing an abundance of fresh fruits and vegetables that include table grapes, raisins, apricots, plums, nectarines, peaches, persimmons, oranges, lemons, grapefruits, pomegranates, nuts, avocados, cotton, wheat, carrots, and a variety of other truck crops. All this is possible due to fertile soils and an extensive network of irrigation facilities.

FIGURE 1: Regional Location

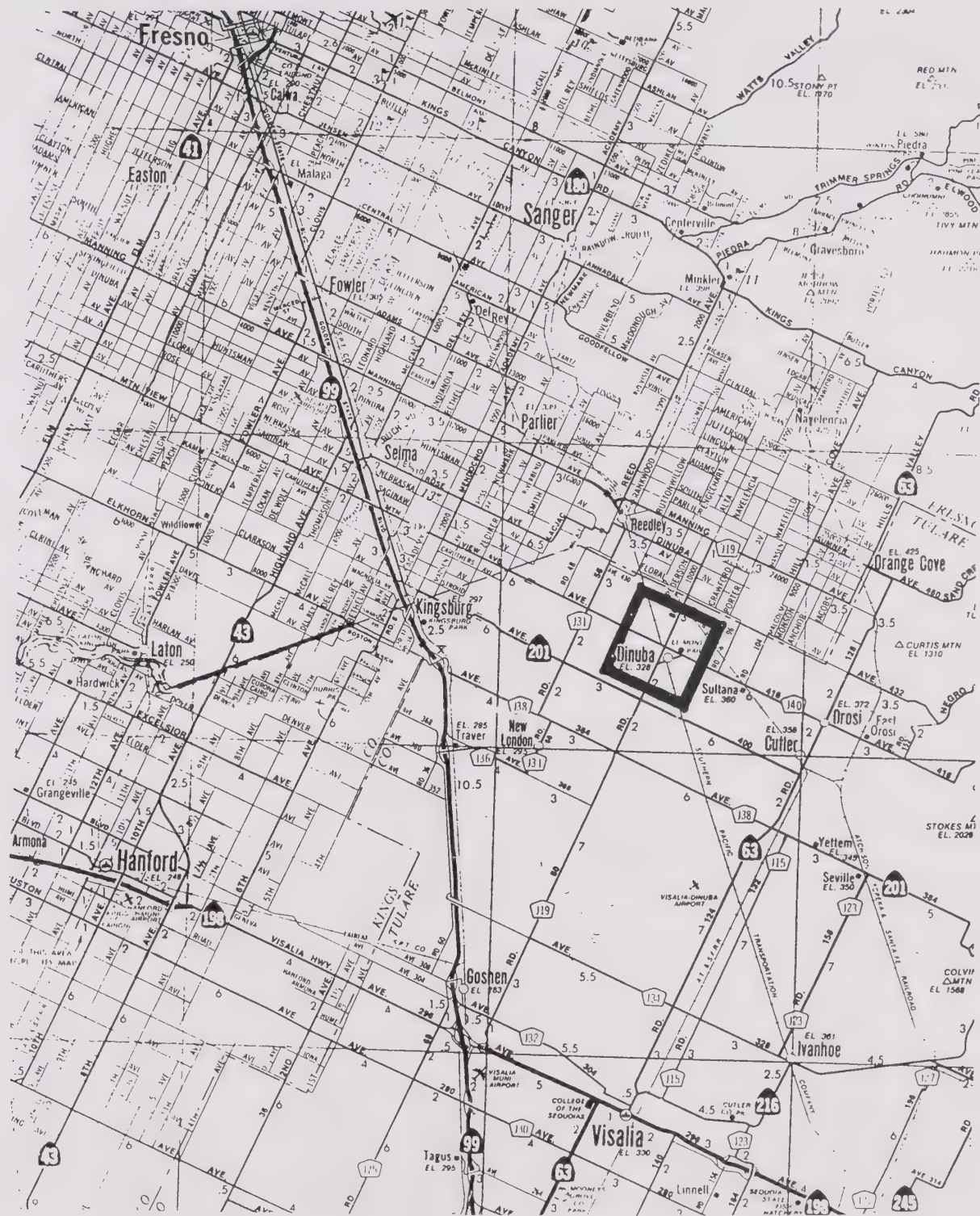
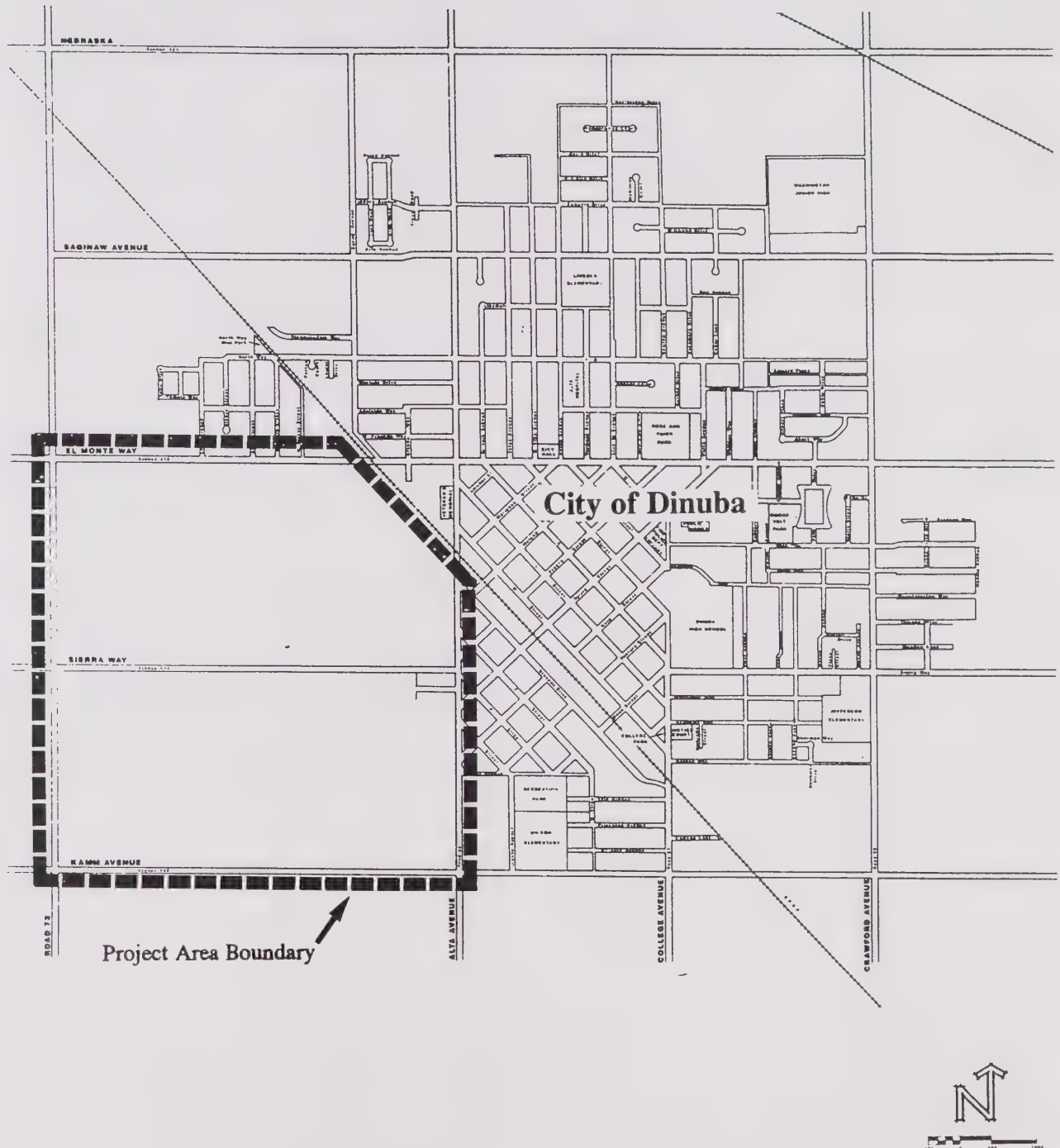


FIGURE 2: Plan Area Location



The primary water feature in the Dinuba area is the Kings River, which lies approximately five miles to the west. Streams and creeks have since been modified in favor of irrigation canals to the extent that there is virtually no trace of natural waterways.

To the northeast of Dinuba are the only significant topographic features in the immediate area, -- Smith Mountain and Burris ("D") Hill. Both are granitic outcroppings that have resisted the erosional affects of weather over time.

The history of Dinuba has been shaped by its agricultural heritage, by the Alta Irrigation District and its system of canals and ditches, and the railroads that have served the area (Southern Pacific and the Santa Fe).

Highways serving the Dinuba area include California highways Freeway 99 (12 miles east), Freeway 198 (14 miles south), 201 (one mile south), 63 (6 miles east) and Tulare County Highways J19 (Road 80/Alta Avenue) and J40 (Avenue 416/El Monte Way). Major airports are located in Fresno and in Visalia. Rail passenger service is available in Fresno or Hanford (28 miles southwest).

Since its founding in 1888, Dinuba has had slow but consistent growth, increasing from 970 persons in 1910 to a current population of 13,075. Among the eight incorporated cities of Tulare County, Dinuba ranks fourth in population. During the 1980's Dinuba's population grew at an average annual increase of 198 persons per year, or a 2.0% average annual growth rate.

The following table shows historical growth and population projections for the City.

Table 1

Population Projections
City of Dinuba

<u>Year</u>	<u>Population</u>	
1940	3,790	
1950	4,961	
1960	6,103	
1970	7,917	
1980	9,907	
1985	10,750	
1987	11,295	
1990	11,950	Actual
1995	13,860	Projected
2000	15,680	
2010	20,070	

Source: Land Use Associates

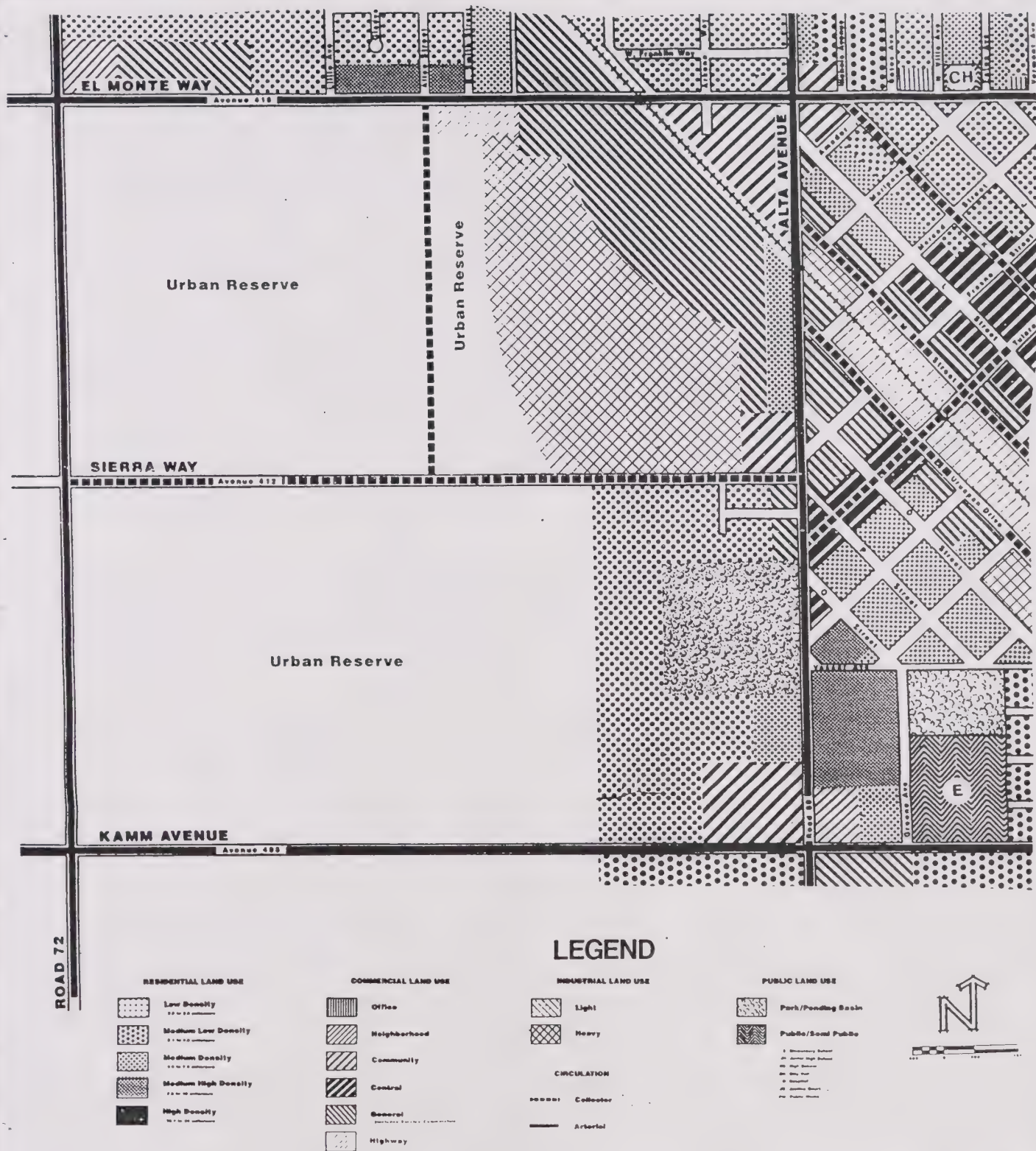
The average annual growth rate in Dinuba is expected to remain strong over the next decade reflecting in-migration to the central San Joaquin Valley and the availability of jobs and housing. The table reflects an average annual growth rate of 2.5% between 1990 and 2010.

Based on the above population projections, the City is growing by 2,605 residents to the year 2000 and by 6,995 residents by 2010. Considering the factors of population growth, family size, and vacancy rates, the City will need an additional 883 residential units by the year 2000 and 2,370 additional units by 2010.

2.4 Adopted Plans and Policies

The Dinuba General Plan - The portion of the Dinuba General Plan for the southwest area is shown in Figure 3. Only a portion of the plan area is designated for urban development with the balance shown as Urban Reserve. North of West

FIGURE 3: Dinuba General Plan



Sierra Way, plan designations are for industrial, general commercial, community commercial, and multi-family residential use. South of Sierra Way, the major designation is for medium density, single-family development, combined with community commercial, multi-family, and open space uses. A major feature of the general plan is the proposal for a large recreation park combined with a drainage basin to serve the downtown area.

The specific plan is not consistent with the general plan and would serve to amend the general plan for this area of the community.

Redevelopment Planning - The City has established a redevelopment agency and adopted the Downtown Urban Design Plan. The purpose of the plan is to reverse the declining economic environment of the downtown and to maintain the area as a commercial, cultural, and aesthetic center of activity. The redevelopment process will continue to be an important development tool not only in the downtown but elsewhere. Within the plan area, approximately 70 acres adjacent to Alta Avenue is within the redevelopment project area.

Tulare County Policy - Development policy in the unincorporated area around Dinuba is controlled by Tulare County. The Comprehensive Policy Plan of the County of Tulare contains policies which guide growth in unincorporated areas, includes the adopted land use plans for each incorporated city in the County, and sets forth the framework for city/county cooperation in land use matters.

The Urban Boundaries Policies define the ultimate Urban Area Boundary (UAB) around incorporated cities. The Dinuba UAB defines the area under county jurisdiction within which Dinuba's concerns are to be given serious consideration as part of the County's land use review process. Within the UAB is the Urban Development Boundary (UDB) which defines the twenty-year planning area around the city in which the County and City have established a program to coordinate plans, policies and standards. Urban development is to occur only within the incorporated city limits, with certain exceptions. Within the UDB, development proposals are referred to the City for annexation according to adopted plans. If the City cannot, or will not, annex, Tulare County will consider the proposal on its merits. The UDB, with its 20-year growth representation, is generally considered the city's sphere of influence for annexation purposes.

The Local Agency Formation Commission (LAFCO) is a state-mandated agency which must approve all annexations, district formations and agricultural contracts

in the City. The 20-year UDB is generally considered the limit to annexation, but occasionally annexations are approved outside this boundary but never outside the larger urban area boundary.

Figure 4 shows the existing city limits within the plan area and the LAFCO sphere of influence. Approximately 130 acres adjacent to Alta Avenue are now within the city limits and contain the Ruiz Foods manufacturing plant, 18 single-family homes, a church, limited service commercial uses, and agricultural land. Approximately 400 acres are within the LAFCO SOI.

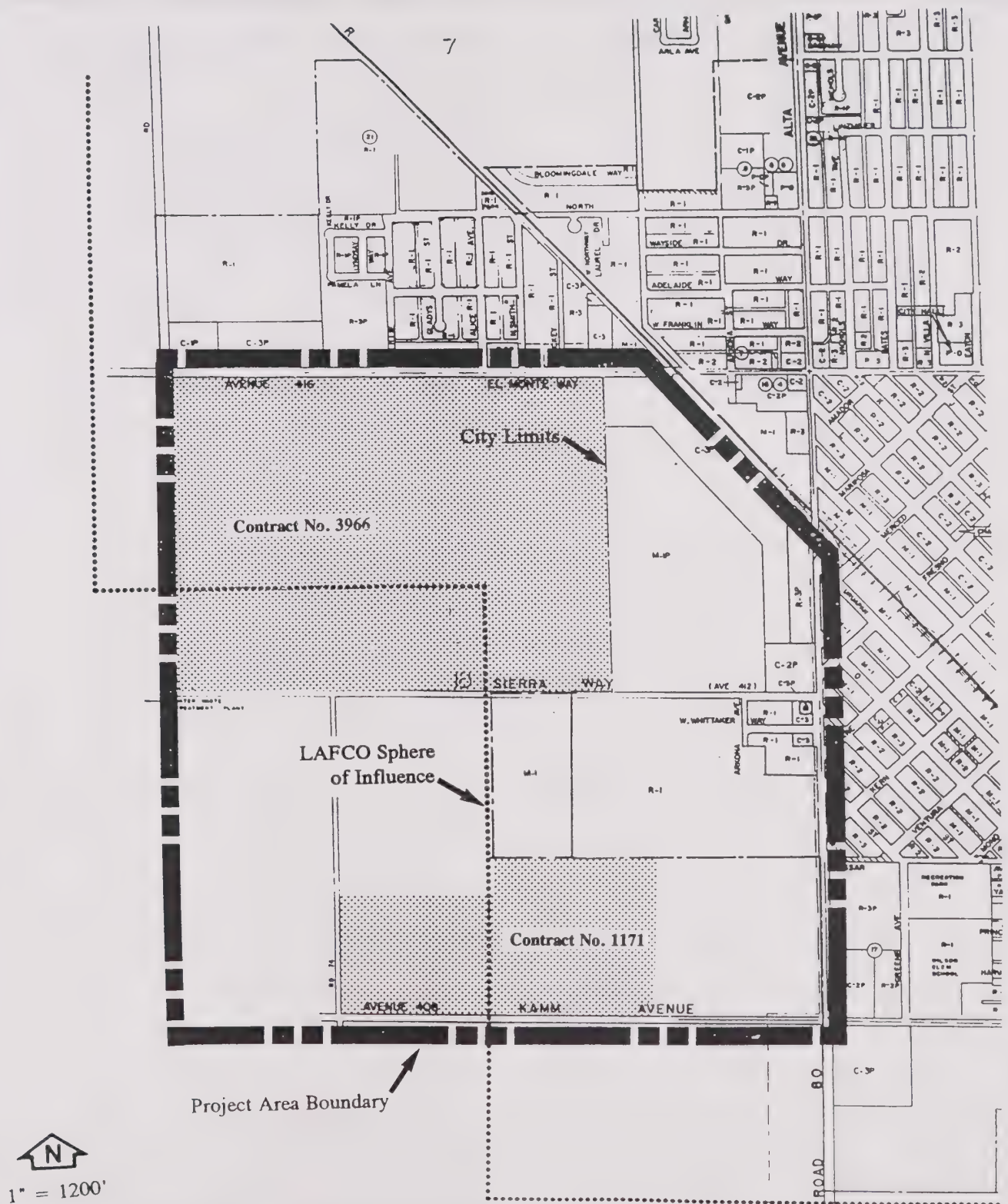
Williamson Act Lands are subject to a 10-year, annually renewing contract with the County intended to prolong agricultural production. The project area includes two Williamson Act Contracts which total 290 acres also shown on Figure 4. Contract No. 3966 north of West Sierra Way was protested by the City on formation and can be cancelled upon annexation. Contract No. 1171 adjacent to Kamm Avenue was not protested nor has a notice of non-renewal been filed.

2.5 Industrial Development Objectives

The industrial development objectives for the plan area are based on the report Target Industry Identification for Tulare County prepared by the firm of Kolzow & Associates in 1989. The report identifies major industrial sectors with a strong likelihood for location in Tulare County. Kolzow & Associates used the following factors in the target industry screening process:

- industry growth rates, both historical and projected
- industrial location requirements matched with the locational attributes of Tulare County.
- industrial linkages with the basic resources and existing industries in the area.

FIGURE 4: City Limits, Sphere of Influence and Williamson Act Contracts



The Kolzow report recommended the following industry types for the Tulare County market.

- Electronic Computing Equipment
- Semiconductors (production/assembly)
- Electronic components
- X-ray apparatus
- Process control instruments
- Optical instruments
- Biological products
- Radio and TV Equipment
- Commercial printing
- Wood Office furniture
- Plastic products, including packaging
- Surgical and medical instruments
- Cheese plants
- Frozen specialties and food preparation
- Wholesale trade, distribution centers

The City of Dinuba has further refined this list and will specifically target food specialties, a cheese plant, packaging, and manufacturing such as wood or plastic products.

CHAPTER 3

LAND USE ELEMENT

This section summarizes existing land use conditions in the plan area and presents objectives, policies and implementation measures designed to promote logical and orderly development.

3.1 Existing Conditions

Existing land uses within the plan area are illustrated on Figure 5 and summarized in the following table.

Table 2

Existing Land Uses

<u>Land Use</u>	<u>Acres</u>	<u>%</u>
Agriculture	574.0	91.7
Commercial	2.0	0.3
Residential	12.0	1.9
Industrial	20.0	3.2
Streets/Railroad r.o.w.	18.0	2.9
	626.0	100.0%

Source: Land Use Associates

The major land use is agricultural, primarily in orchards. There is one 20-acre parcel in industrial use. Ruiz Foods opened in 1991 and currently employs more than 1,000. There are also three small parcels in service commercial use on Alta south of West Sierra Way. A single-family subdivision and church are located southwest of the West Sierra Way/Alta intersection.

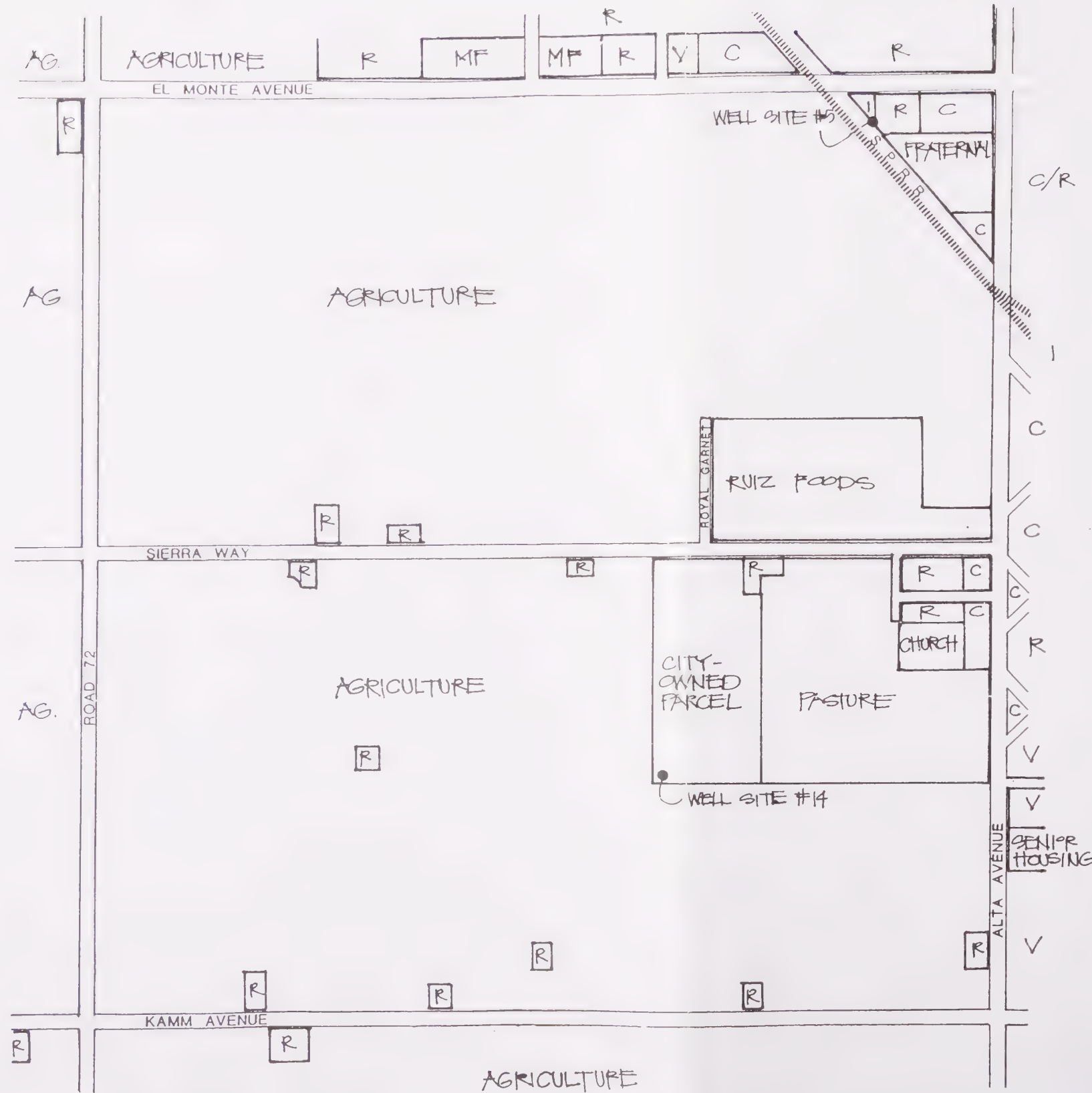
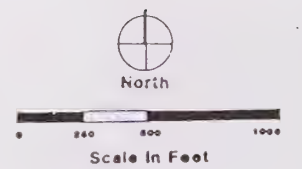


FIGURE 5: Existing Land Use

Legend

- AG Agriculture
- R Residential
- C Commercial
- I Industrial
- V Vacant
- MF Multifamily



SOUTHWEST DINUBA SPECIFIC PLAN

LAND USE ASSOCIATES

July 1992

Land uses surrounding the plan area include residential and commercial uses to the north and industrial and service commercial uses to the east. Agricultural lands surround the plan area on the south and west.

A spur track of the Southern Pacific Railroad, now operated by the San Joaquin Valley Railroad Company of Exeter, bounds the plan area on the northeast and can provide spur track service to industrial uses.

3.2 Land Use Map

Figure 6 is the Specific Plan land use and circulation map.

Table 3

Land Use by Acres

<u>Land Use</u>	<u>Acres</u>	<u>%</u>
Light Industrial	372.7	59.5
General Commercial	31.5	5.0
Office Commercial	29.8	4.8
Medium High Density Residential	17.0	2.7
Day Care Facility	2.0	0.3
Park/Ponding Basin	80.0	12.8
Streets/Railroad r.o.w.	89.0	14.2
Medium Density Residential	<u>4.0</u>	<u>0.6</u>

626.0 acres

Source: Land Use Associates

Definitions of land use classifications used in this plan follow. Zoning consistent with each land use classification is also presented. Standards for the development of each land use classification are in large measure embodied in the consistent zoning district.

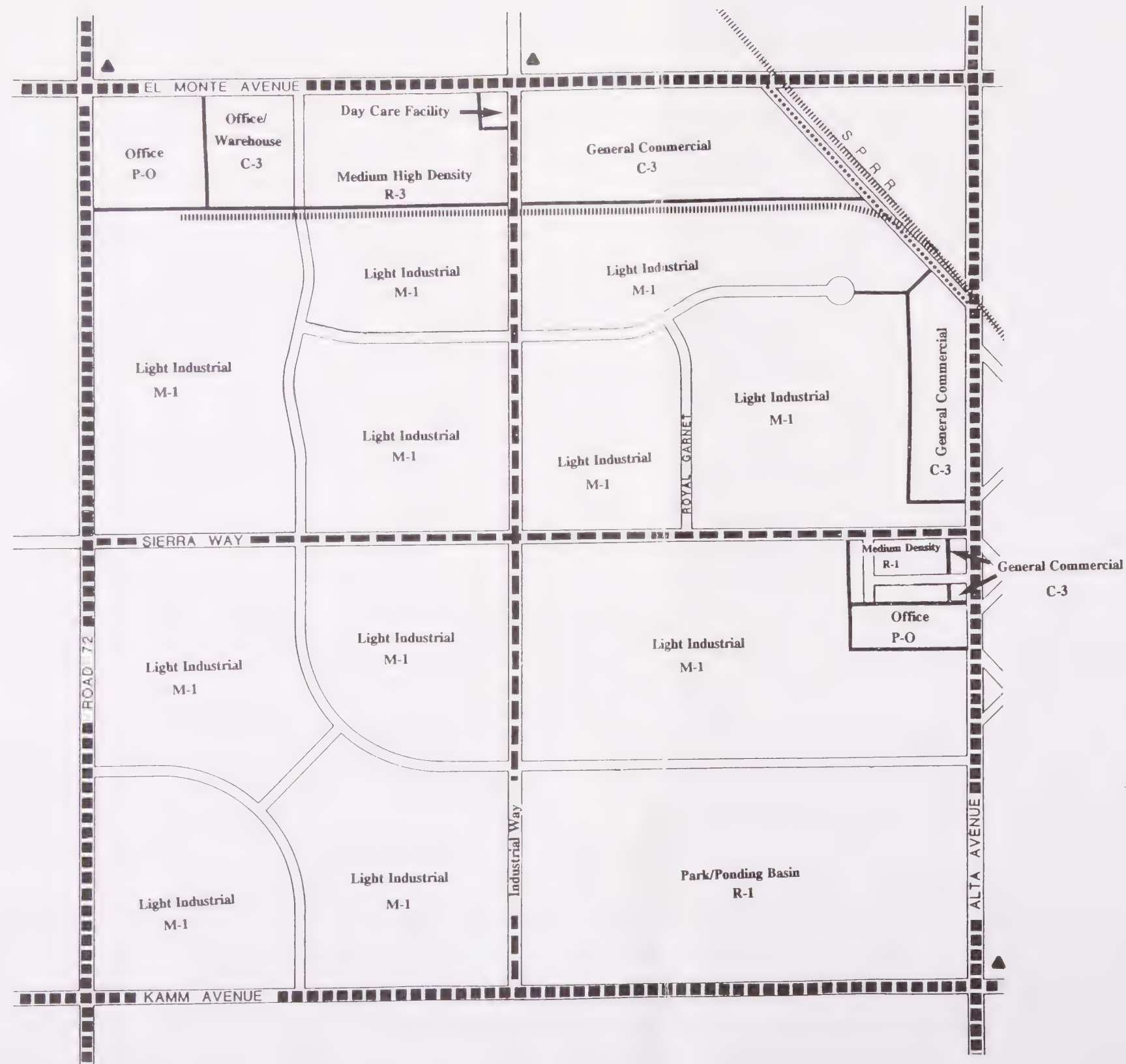


FIGURE 6: Specific Plan Map

Arterial ■■■■■■
 Collector - - - - -
 Proposed Traffic Signal ▲



SOUTHWEST DINUBA SPECIFIC PLAN

LAND USE ASSOCIATES
 July 1992

- (a) Light Industrial - This category establishes light industrial areas where uses such as fabricating, assembly, research and development, electronics, low intensity warehousing and other such similar industrial uses are appropriate. All work, materials, and equipment storage is generally conducted indoors. Light industrial is appropriate where the site is visible from residential areas or major streets. Special landscaping, enclosures and other site development standards are appropriate.

Industrial park development is intended on larger parcels to create distinct districts of industrial, office, and support uses. Consistent zoning is M-1.

- (b) General Commercial - This designation provides for commercial areas with a wide range of retail and service activities along major traffic corridors. Consistent zoning is C-3.
- (c) Office Commercial - This designation provides for office development which includes medical, dental, law, or other professional offices. Commercial uses contemplated as part of this category include business support services and support restaurant and medical services. High density residential uses are also allowed in the office commercial designation subject to a Conditional Use Permit. Consistent zoning is P-0.
- (d) Medium-High Density Residential - (7.6 - 15.0 units/gross acre). this category provides for a land use pattern characterized predominantly by multiple family residential development. The typical residential pattern ranges from 4,500-square foot lot sizes to duplex and large-scale apartment development. Areas designated medium-high density residential are often near or adjacent to single-family neighborhoods. In such cases, it is important that multi-family projects are developed in a manner that reduces or eliminates potential adverse effects on single-family uses. Consistent zoning is R-3.
- (e) Day Care Facility - This designation provides for development of a professional day care facility for the day-time care of adolescent and pre-adolescent children. The facility is primarily intended to serve employees within the specific plan area. Consistent zoning would be R-1 with a Conditional Use Permit required.

- (f) Park/Ponding Basin - The facility shown on the plan map is consistent with the Storm Drain Master Plan and would provide a flood control facility for the downtown Dinuba area. In conjunction, a large scale community park would be developed. Consistent zoning is R-1 with a Conditional Use Permit required.
- (g) Medium Density Residential (4.6 - 7.5 units/gross acre) - The medium density residential category provides for a land use pattern of predominantly single-family development and associated uses. Consistent zoning is R-1.

3.3 Policies and Implementation Measures

The following policies and implementation measures will guide development in the plan area.

3.3.1 Improve the Economic Base

1. Pursue a program of tax base expansion to include both industrial and market-area commercial uses. To this end, the City should:
 - (a) Reserve sufficient space for industrial and commercial uses, recognizing greater land requirements due to methods of operation and marketing needs.
 - (b) Provide variety in locations to avoid creating a monopoly on the land market.
 - (c) Develop policies and regulations to protect industrial and commercial areas from incompatible uses.
2. Designate an industrial area to promote the orderly development of new industry in a controlled environment. Alternatives for ownership and operation include:
 - (a) The private sector,

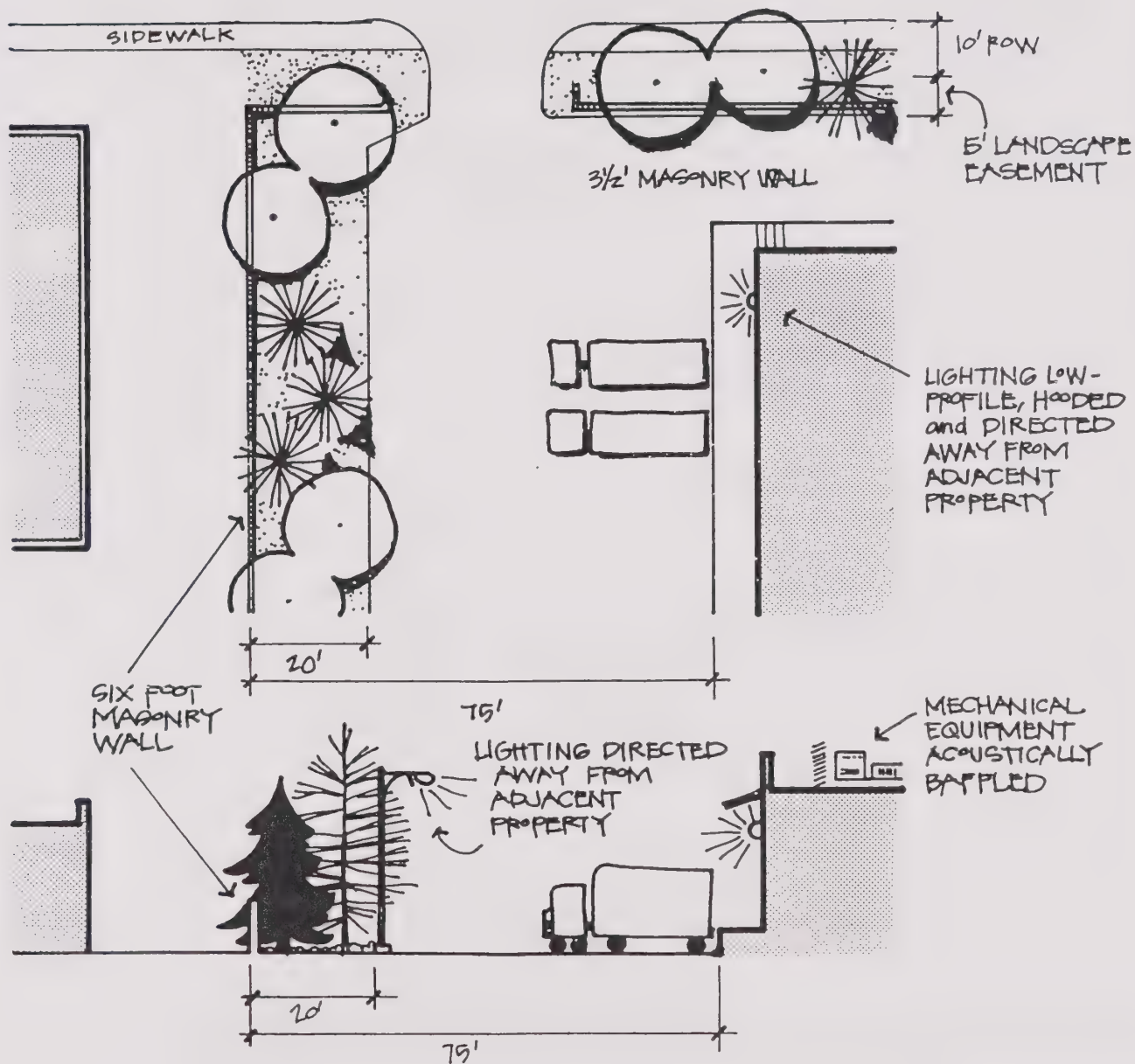
- (b) A non-profit corporation, or,
 - (c) By the City through its redevelopment agency or other means.
3. The City shall minimize the adverse environmental effects of industrial growth by recruiting industries which can reduce pollution impacts to acceptable levels; by locating industry in areas where growth will have the least impacts; and by requiring adequate buffering to protect adjacent land uses.
 4. Use the redevelopment process as a tool to implement the General Plan and to revitalize the urban area. The primary purposes of redevelopment are to remove blighting influences; provide urban support services; and to rehabilitate and enhance residential, commercial, industrial, and institutional uses.

3.3.2 Location and Intensity of Land Uses

1. The land within the plan area shall be developed in an efficient and cost-effective manner. A potential development pattern that conforms with this policy is shown on the specific plan map. The depicted pattern is intended to be advisory only; alternative development patterns may be proposed for the plan area.
2. Locate industry with access to major streets, truck routes, and rail service. The City should plan for the extension of rail spurs to planned industrial areas and assist in their extension.
3. Ensure that industrial development creates no significant off-site impacts concerning access and circulation, noise, dust, odors, visual features, and hazardous materials that cannot be adequately mitigated.
4. Apply the general commercial designation along arterial streets to provide commercial support for nearby community and central commercial uses as well as industrial areas. General commercial includes freestanding uses which do not fit well in unified centers as well as service and highway commercial uses.

5. Provide office land use unified designations which allow construction of new office unified centers, the redevelopment of existing areas to office use, and the conversion of older homes to offices along major streets.
6. Provide adequate separation and buffering between industrial or commercial uses and residential uses as shown on Figure 7.
7. Where new residential development is proposed adjoining existing commercial or industrial development, the residential developer shall be required to provide an architectural transition, if needed, through the Conditional Use Permit process. The transition may include such provisions as building setbacks, landscaping and masonry wall requirements to benefit future residents.
8. The Zoning Ordinance and Subdivision Ordinance are the primary tools available for guiding the location and intensity of land uses. State law requires that the Zoning Ordinance and the design and improvement of all subdivisions be consistent with the specific plan. The following measures are designed to implement the objectives of this subsection:
 - (a) Upon adoption of the specific plan or any subsequent amendment, the City will undertake any amendments to the Zoning or Subdivision Ordinances necessary to maintain consistency between them and the plan. An amendment to the Zoning Ordinance or Subdivision Ordinance is consistent with this plan if it will further the objectives and policies of the plan.
 - (b) The City will only approve amendments to the Zoning Ordinance, conditional use permits, variances, and subdivisions of land, together with the provisions for their design and improvement, that are consistent with this plan.
 - (c) Upon the comprehensive update of the Zoning Ordinance, zoning districts within the specific plan may be changed to new districts most consistent with the intent of the specific plan.
9. Provide for a 5 to 10-acre fire station/corporation yard within the M-1 district. The exact location will be determined following completion of the Fire Service Master Plan.

FIGURE 7: Interface Between Industrial/Commercial and Residential Uses



3.3.3 Growth Management and Annexation

1. In cooperation with Tulare County, adopt an Urban Development Boundary to include all territory designated for urbanization on the General Plan Map for purposes of administering the County's Urban Boundaries Policies.
2. Future urbanization should be phased to encourage contiguous land use where possible. This includes concentration on the "in-filling" of vacant lands bypassed by urbanization.
3. The premature conversion of producing agricultural lands to urban purposes is discouraged. Steps to reduce such conversion include phased growth, programmed extension of urban services, and use of Williamson Act Contracts where urbanization is not anticipated within the next 10-year period.
4. In cooperation with the Tulare County LAFCO, adopt and maintain a Sphere of Influence consistent with State annexation laws:
 - (a) The City will proceed with the orderly annexation of land within the plan area. The first area to be considered for annexation will be the area bounded by El Monte on the north, the existing City limits on the east, and West Sierra Way on the south. The City will consider annexations in the remaining portions of the plan area at such time as service capabilities and development activity warrant.
 - (b) The City will consult with property owners and LAFCO prior to and during the annexation process. The purpose of the consultation will be to gain the understanding and support of the property owners and LAFCO for the annexation and to address any questions or concerns that may exist.
5. To protect unincorporated land from inappropriate or premature development, the City will request that Tulare County:
 - (a) Maintain agricultural zoning on all such land to prohibit division of the land into lot sizes not conducive to agricultural purposes.

- (b) Refer applications for development to the City for review and possible annexation. The City's review will determine if the conditions described below are met. If the application conforms with these conditions the City will request that the County not process the application further and refer the applicant to the City for annexation and processing. The conditions are:
- The proposed type of development is consistent with the specific plan for the land on which the development is proposed.
 - The proposed development can be adequately served by City services.
 - The proposed development can be served by area streets without substantially altering existing traffic patterns or overloading the street system.
6. Where an application submitted to Tulare County for development of unincorporated land does not meet the conditions identified above, the City will advise Tulare County that the proposed development is premature, is not consistent with the specific plan, and will request that the County not process or approve the application.

3.3.4 Design and Performance of Development

1. Require site plan review procedures for all multi-family, commercial, and industrial development, including provisions for building setbacks, lot coverage, parking, access and circulation, outdoor lighting, signage, and landscaping.
2. Ensure that all commercial and industrial development is attractive and high quality design to enhance the image of the city.
3. Promote a citywide street tree planting program which enhances views and is scaled in relationship to the function of the roadway. Landscaped areas should be located and designed to maintain views for traffic and pedestrian safety.

4. As primary entrances to the City, Alta and El Monte should reflect higher standards of development. To promote these higher standards, a boulevard overlay district shall apply to Alta and El Monte. The BA-20 district contains provisions for minimum building setbacks, landscaping, sidewalk pattern and street furniture.
5. No outdoor advertising billboards shall be allowed on Alta Avenue or El Monte Way within the limits of the specific plan boundary.
6. West Sierra Way and proposed Industrial Way, collector streets which pass through the industrial area, shall receive special design treatment to reduce aesthetic impacts and traffic concerns.
 - (a) The minimum building setback from the right-of-way line shall be 40 feet.
 - (b) There shall be a BA-10 district established on each side of the roadway.
 - (c) The number of driveway approaches shall not be greater than two for individual parcels; efforts should be made to consolidate driveways along common property boundaries, where possible.
7. Establish coordinated and distinctive signage, accent plantings and paving materials for entry into the industrial area.
8. All exterior on-site utilities including, but not limited to, drainage systems, sewers, gas lines, water lines, electrical, telephone, and communications wires and equipment shall be installed and maintained underground. Items which must be placed above ground for function and safety reasons shall be screened from view.
9. On-site underground utilities shall be designed and installed to minimize the disruption of off-site utilities, paving and landscape during construction and maintenance.
10. Roof-mounted mechanical equipment shall be hidden from view by building parapets of equal height. If building parapets do not provide adequate screening when observed from adjacent buildings, mechanical

equipment shall be screened by an unobtrusive device that will appear to be an integrated part of the overall architectural design.

11. All utility and other public easements shall be landscaped and maintained.
12. Parking lots shall be well landscaped such that 50% of the parking lot is shaded within 15 years.
13. Compact parking stalls may account for a maximum of 30% of all required parking stalls.
14. The following signs are prohibited:
 - Signs on or affixed to trucks, vans, automobiles, trailers or other vehicles which advertise or provide directions to a use or activity not related to the ordinary rendering of service from such vehicles.
 - Pole signs, pylon signs or freestanding signs which exceed four feet in height.
 - Portable or freestanding signs not permanently affixed, anchored, or secured to the ground or a structure on the lot it occupies, including tailored signs. Sandwich boards are prohibited.
 - Roof signs erected, constructed and maintained upon or over the roof of any building are prohibited.
 - Other prohibited signs include advertising signs, billboards, inflatable signs or balloons, inflatable animals, and magnetic signs.

CHAPTER 4

CIRCULATION ELEMENT

This section establishes objectives, policies and implementation measures for streets and rail transportation within the plan area. The primary goal for circulation and transportation is to provide for a street and rail system which moves people and goods in and through the plan area in an orderly, safe, and efficient manner.

4.1 Existing Conditions

Streets

Regional access to the City of Dinuba is provided by Freeway 99 via Mountain View/Avenue 416/El Monte from the west and north and Road 80/Alta from the south. Both El Monte and Alta are two-lane arterials.

Direct access to the plan area is via El Monte, Alta, Kamm and West Sierra Way. General plan designations for these roadways, and their planned rights-of-way are:

<u>Arterials</u>	<u>Planned Right-of-Way (feet)</u>
Alta Avenue	96
El Monte Way	96
Kamm Avenue	84
<u>Collectors</u>	
West Sierra Way	84

All of the roadways are in fair-to-good condition. The Alta/El Monte and Alta/West Sierra intersections are signalized.

Truck Routes

Designated truck routes include El Monte Way, Alta, Kamm, and West Sierra.

Rail Transportation

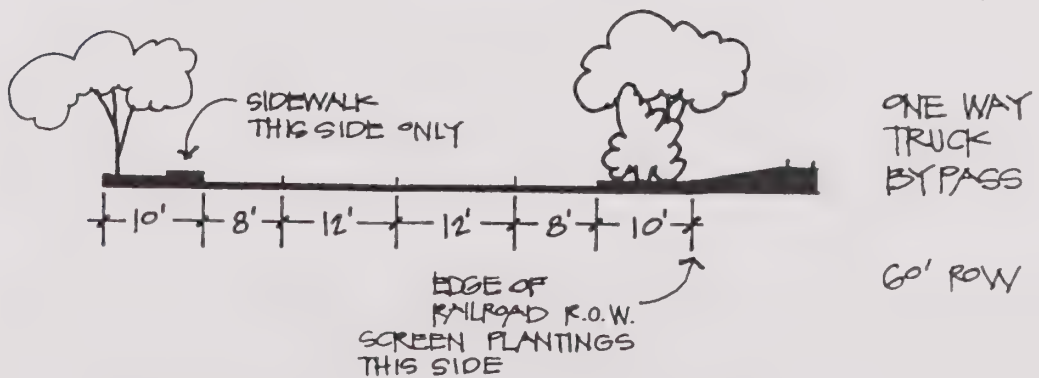
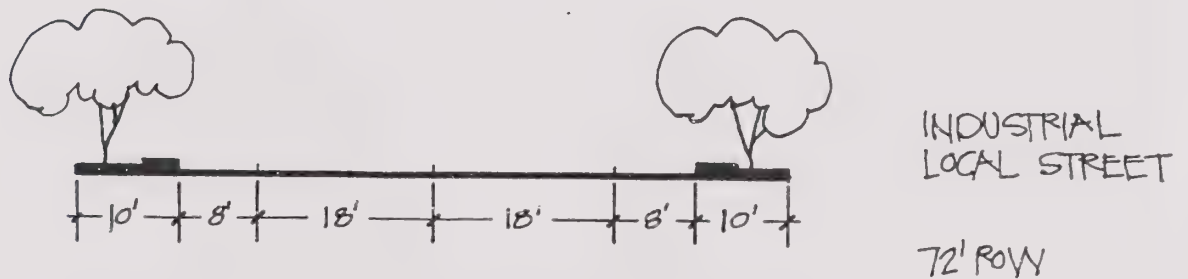
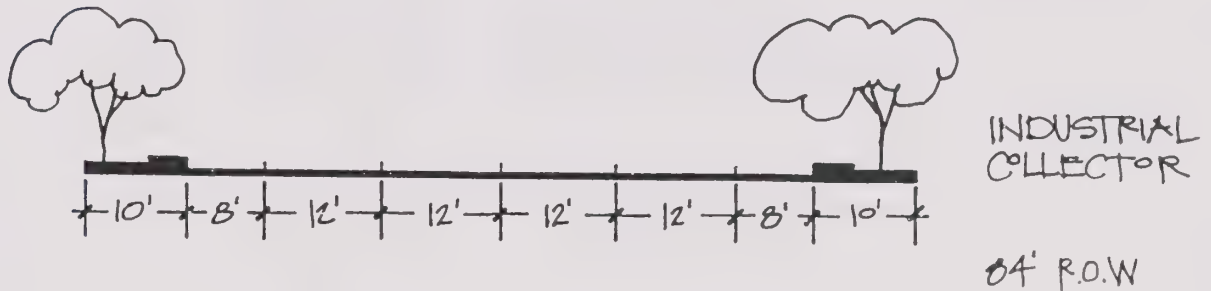
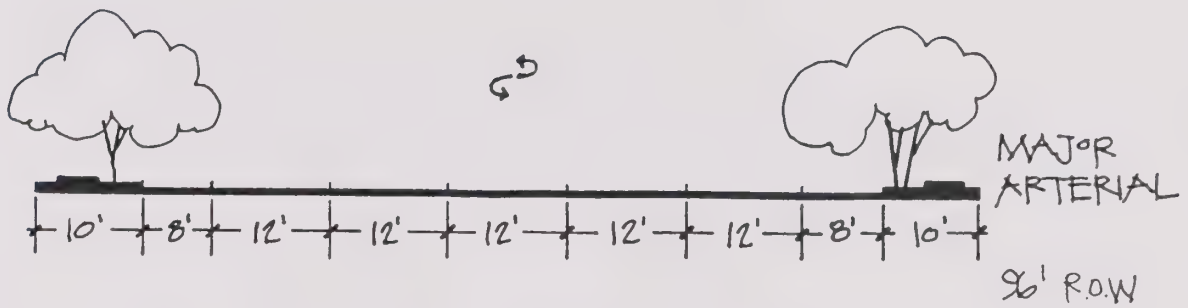
Rail transportation to the plan area is provided by the San Joaquin Valley Railroad Company, a private company which has purchased the Southern Pacific spur track which bounds the plan area on the northeast. The tracks have an average of two runs per week during the year, but may increase to four runs per week during harvest season.

4.2 Definitions and Standards

The following are definitions and standards for the street system as shown on Figure 8.

1. Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the urban areas and connect with important county roads and state highways. They also provide for the distribution and collection of through traffic to and from collector and local streets.
2. Collector streets provide for traffic movement between arterial and local streets, traffic movement within and between major activity centers, and limited direct access to abutting properties.
3. Local industrial streets provide for direct access to abutting properties and for very localized traffic movements. Local industrial streets are to be developed within a 72-foot right-of-way.
4. A one-way truck bypass connects El Monte Way with Alta parallel and southwest of the Southern Pacific Railroad. The purpose of the bypass is allow truck and auto traffic to move directly from east-bound El Monte to south-bound Alta while avoiding the El Monte/Alta intersection, thus reducing turn movements and congestion. The bypass is to be developed as a two-lane roadway within a 60-foot right-of-way.

FIGURE 8: Roadway Cross Sections



4.3 Policies and Implementation Measures

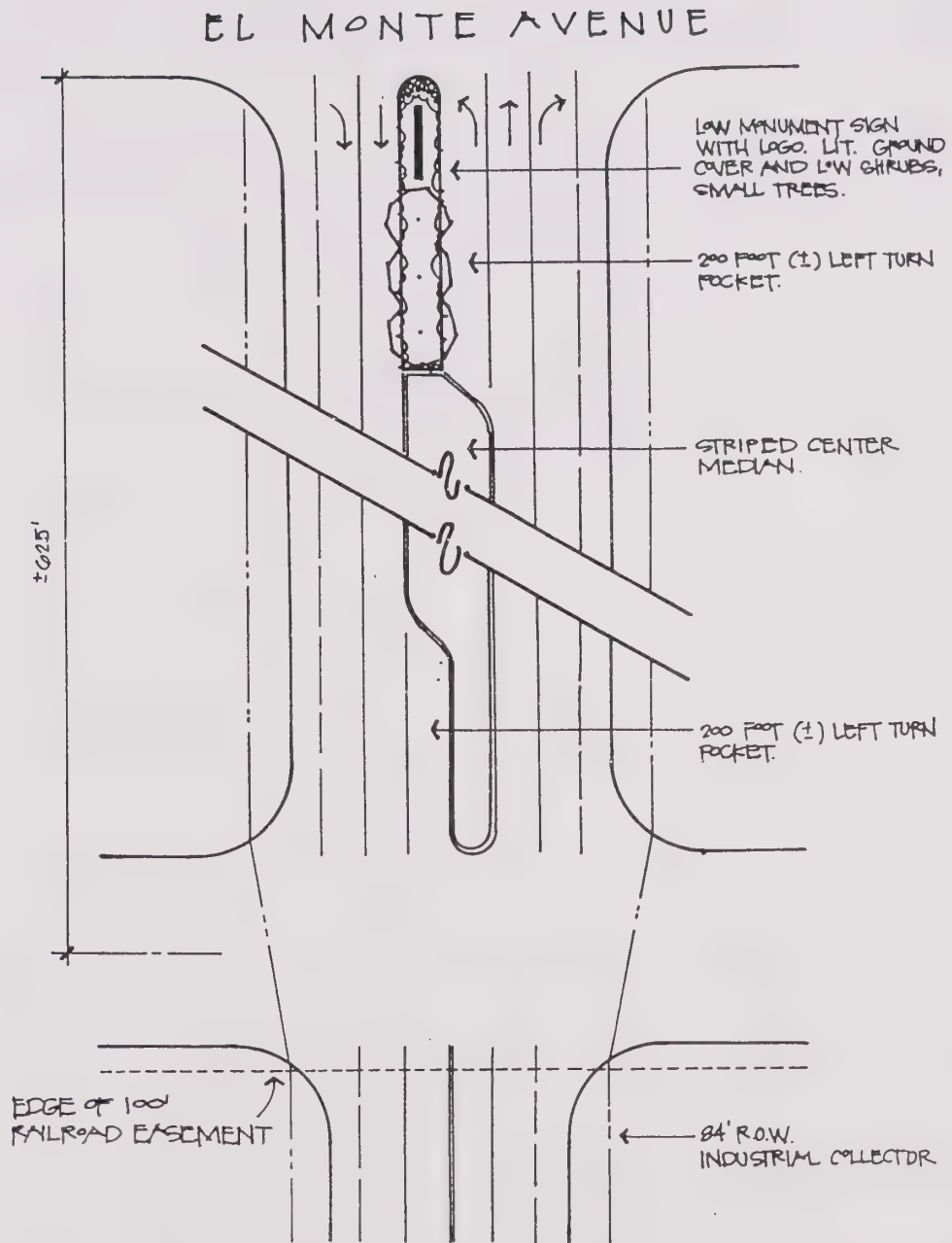
4.3.1 Circulation

1. Promote the improvement of El Monte Way as the primary transportation access to the City from Highway 99. To this end, cooperate with Tulare County, Fresno County, and Caltrans to prepare plan lines and secure funding for right-of-way, additional lanes, and signalization.
 - (a) The ultimate development of El Monte should include four travel lanes from Highway 99 to the City.
 - (b) Encourage Caltrans to improve directional signage to Dinuba from major Highway 99 exits.
2. The major street system for the plan area is:

<u>Arterials</u>	<u>Planned Right-of-Way (feet)</u>
Alta Avenue	96
El Monte Way	96
Kamm Avenue	84
Road 72	84
<u>Collectors</u>	
West Sierra Way	84
Industrial Way (new street)	84

3. The intersection of El Monte and Industrial Way shall be developed as shown on Figure 9 as the major entrance to the plan area and to provide for safe and efficient traffic movement.
4. All streets within the plan area shall be designated as truck routes.
5. The City will, as a condition of land development, require the dedication and improvement of streets in accordance with the standards of the specific plan and the City's Improvement Standards.

FIGURE 9: Entrance at El Monte/Industrial Way



6. The City will adopt official plan lines for all major streets within the plan area which do not already have their full rights-of-way.
7. Direct access from an arterial or collector street to abutting property will be controlled through design requirements which provide for access to other roads or limits on the number and/or location of points of direct access.
8. Turnaround facilities shall be provided on parcels having access to arterials or collectors so that vehicles do not back out onto roadways.
9. Direct access will be permitted from local industrial streets to abutting property.
10. Curve widening will be required at intersections to accommodate interstate trucks in accordance with appropriate Caltrans standards.
11. Provide signalization and intersection improvements at the following locations:
 - Industrial Way/El Monte Way
 - Kamm/Alta
 - Road 72/El Monte Way
 - Full arterial intersection improvements at El Monte Way/Alta.

4.3.2 Rail Transportation

1. Provide a 100' wide easement for rail spurs at locations as shown on the specific plan map.
2. Subdivisions within the plan area should be designed to facilitate access to rail transportation.
3. The City will provide assistance to industrial developers in designing projects to maximize rail access.

CHAPTER 5

PUBLIC FACILITIES AND SERVICES ELEMENT

The purpose of this section is to establish objectives, policies, and implementation measures for public facilities and services. The primary goal for public facilities and services within the plan area is to provide efficient, effective and responsive public facilities and services for industries.

5.1 Existing Conditions

Utilities - Electrical service is provided by Pacific Gas and Electric Company and natural gas is provided by Southern California Gas Company. Telephone service is provided by Pacific Bell Telephone Company. Home Town Cable TV provides cable television service. These utilities forecast no difficulties in meeting the demands of projected growth.

Police Protection - Police protection services are provided by the City of Dinuba Police Department. The staff includes twenty-one full-time and eleven reserve officers. Official vehicles include two unmarked units (one chief vehicle and one detective vehicle) and seven marked patrol units. A cooperative agreement for supplemental services exists between the City of Dinuba and the Tulare County Sheriff's Department. Service to the unincorporated area is provided by the Tulare County Sheriff's Department Orosi Sub-station.

Fire Protection - Fire protection services are provided by the City of Dinuba Fire Department which is manned by eight full-time and 20 volunteer firefighters, two fire engines, and three ambulances. The average response time to fire calls is 3-5 minutes. The current insurance rating of six by the Insurance Service Office (ISO) covers the entire City. As new areas develop, water systems are required to meet fire flow. A cooperative agreement exists with Tulare County Fire Warden/California Division of Forestry and medical units. Fire protection for the surrounding county area is provided by three California Division of Forestry fire stations.

Sewer and Wastewater Treatment - The City of Dinuba operates a wastewater reclamation facility approximately 1-3/4 miles west of the proposed plan area. The existing facility has a rated capacity of 3.2 to 3.8 million gallons per day. The

average daily effluent is 1.4 million gallons and the peak day is currently 1.7 million gallons per day.

Current loadings are such that the City has little difficulty in meeting its wastewater discharge requirements as the facility is currently managed. Once sludge is extracted from the facility the resultant water processed through the facility is aerated and stored on site in ponds covering approximately 30 acres. Stored water is disposed of through irrigation (on site, alfalfa and oats), evaporation and infiltration (ground water recharge). Existing storage facilities can dispose of and manage 1.8 to 2.0 million gallons per day depending on weather conditions.

The City has purchased an additional 30 acres. The intent is to develop an amount of storage equivalent to existing storage. This will provide adequate storage to carry the community to the general plan "build-out" level of development, assuming a continuation of the types and varieties of development the community has historically experienced.

The City is investigating cooperative agreements with abutting agricultural properties for the disposal of finished wastewater effluent. Facilities will be expanded to allow the City to treat its effluent to Title 22 standards, making the effluent usable for irrigation purposes. All abutting properties have expressed a willingness to accept all available water the City can provide to them.

The City's collection system can adequately serve the entire plan area. Within the existing collection system there are currently no deficient facilities (broken or damaged pipes, capacity issues). The City provides periodic and on-demand system maintenance consisting of repairs as needed and jet flushing, rodding, root removal and video inspection services.

Water Supply - Dinuba is served by seven wells (#3, #4, #5, #9, #11, #12, and #14) that produce 3.4 million gallons of water on an average day and 6.8 million gallons on a peak day. The system has a daily capacity of 10.9 million gallons which results in a 4.1 million gallon per day reserve capacity.

The majority of the City's wells are within or immediately adjacent to the proposed plan area. New wells to be developed will also be immediately adjacent to the plan area. Water system extensions can be made to those properties not currently served with a minimum of difficulty.

One well (#14) was constructed by the City in the plan area in 1991. It has a capacity of 1,500 gallons per minute. Ruiz Foods uses about one-third of the daily production, with the balance going into the grid system. A well site for well #17 has been located at the northeast corner of West Sierra Way and future Road 72.

Two additional wells have been recently drilled by the City. Well #15 is located north of El Monte Way near Road 72. Well #16 is located north of Kamm at Green Street. Both these wells will provide water to the plan area.

All on-line wells meet federal clean water drinking standards with the exception of #12 which exceeds the DBCP standard. Several wells in the City are off-line due to high levels of DBCP. One such well (#5) is located just north of the Southern Pacific tracks outside the plan area. Water from the well could be used for non-food processing or landscaping.

Storm Drainage - The City of Dinuba adopted a Storm Drainage Master Plan in 1989. This plan details drainage system improvements for each basin within the community and establishes a development fee structure to finance needed improvements as each area develops. It is a policy of the Storm Drainage Master Plan that all industrial development utilize on-site drainage basins rather than common basins.

The City has purchased a 20-acre site on West Sierra Way for the purpose of constructing a ponding basin to handle drainage from the Mid-town Area. The specific plan proposes, however, that this 20-acre site be developed with industrial uses, and that an 80-acre site be purchased just north of Kamm in order to provide expanded recreational opportunities.

5.2 Policies and Implementation Measures

1. The water and sanitary sewer systems within the plan area shall be developed in an efficient and cost-effective manner. Potential designs for these systems are shown on Figures 10 and 11. The City's adopted Master Plans for the utilities will govern any proposed system. The depicted design is intended to be advisory only and to supplement the City's Master Utility Plans.

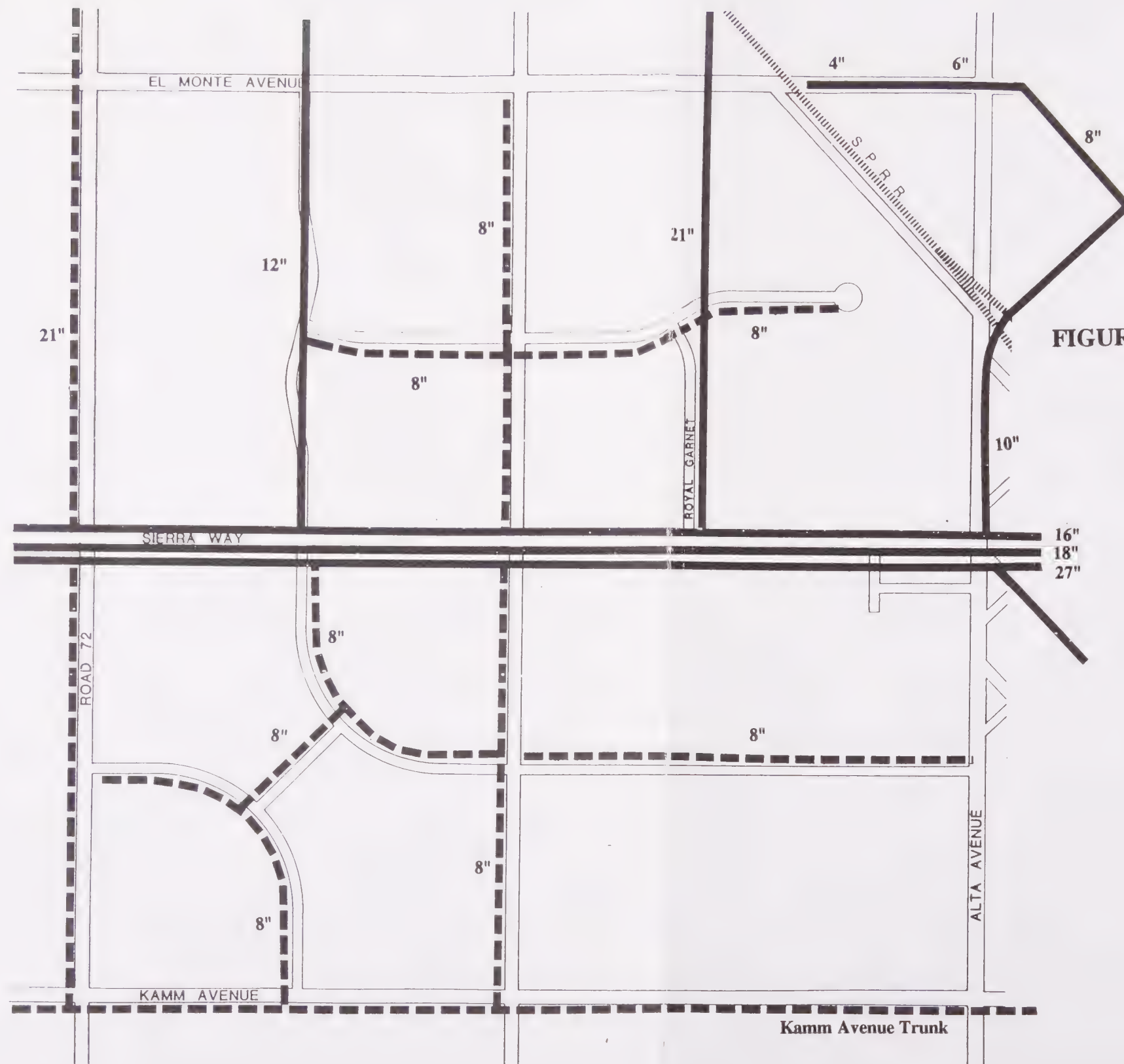


FIGURE 10: Existing and Proposed Sewer Facilities

Existing Sewer Lines —————

Proposed Sewer Lines - - - - -



SOUTHWEST DINUBA SPECIFIC PLAN

LAND USE ASSOCIATES
July 1992



FIGURE 11: Existing and Proposed Water Facilities

Existing Water Lines
 Proposed Water Lines



SOUTHWEST DINUBA SPECIFIC PLAN

LAND USE ASSOCIATES

July 1992

2. The City will require, as a condition of approval of development within the plan area, connection with the City water and sewer systems. The City shall also ensure that the systems can adequately serve the development.
3. The City will require the use of on-site storm water drainage systems for new industrial development in conformance with the Master Storm Drain Plan.
4. The City will maintain an adequate fire department. A 5 to 10-acre fire station/corporation yard is proposed within the plan area in the M-1 district. No specific location is proposed pending the completion of the Fire Service Master Plan.
5. The City will maintain adequate police department staff to protect industrial development within the plan area.
6. The Police Department will consult with developers during the site planning and building design phases of projects to ensure that provisions for adequate police protection and burglary prevention are designed into projects.
7. The Police Department will conduct regular public relations checks with industries in the plan area to ensure that police staff are aware of special police protection needs which might arise.
8. Consider providing public cost-sharing or public services under certain circumstances to encourage desirable and innovative development within the city.
9. Require that new industries which generate high levels of organic or inorganic wastes either pre-treat wastes at the industrial site, or contribute fairly toward the costs of sewage system maintenance.
10. Use special districts as appropriate to provide improvements such as street drainage, open space, fire protection, and other public works projects. Options include the Improvement Act of 1911, the Municipal Improvement Act of 1913, and the Improvement Bond Act of 1915.

CHAPTER 6

ENVIRONMENTAL RESOURCES

This chapter contains policies and implementation measures for open space, conservation, noise and safety as they relate to overall general plan policy. In addition, policy statements are offered on individual project design.

6.1 Conservation, Open Space and Recreation

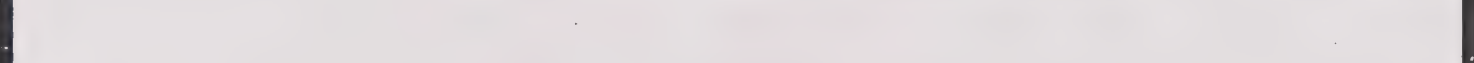
1. The 80-acre park/ponding basin shall be developed in accordance with requirements of the Dinuba Master Plan of Parks with an emphasis on active recreation and large group events.
2. Individual commercial and industrial developers shall be encouraged during site plan review to provide outdoor landscaped areas for the passive enjoyment of employees, including benches and tables.
3. Industrial storm drain areas should be included in contoured landscaped swales on the site when feasible. Where a single drainage basin is constructed, it shall be developed according to requirements of the Storm Drainage Master Plan including landscaping and screening. During site plan review, consideration should be given to developing limited recreation facilities in large basins.

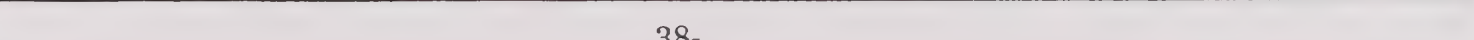
6.2 Flooding

1. The southeast portion of the plan area is subject to flooding in a 100-year event, as shown Figure 12. Development areas shall be graded to elevations sufficient to protect structures from flooding in a 100-year event (building pads a minimum of one foot above flood elevation).

6.3 Water Conservation

1. The storm water management system shall be designed to protect groundwater quality.





2. Methods shall be encouraged for recycling waste water. Reclaimed water may be used for irrigation of greenbelts and median landscaping or as process water.
3. Water conservation shall be a priority consideration in design and construction within the plan area including landscape treatments.
4. Landscaping plans for the project shall emphasize the use of trees and shrubs that are indigenous to the climatic region.
5. The feasibility of using well water from Well #5 high in DBCP content shall be investigated for non-potable purposes.

6.4 Energy Conservation

1. Buildings within the project area shall be subject to energy conservation requirements of Tulare county and the State of California.
2. Natural gas and electricity conservation methods can be readily incorporated into proposed projects during the design phase of development. Consultation with PG&E and Southern California Gas during design will facilitate adapting architectural design with maximizing efficient energy use. Projects shall be in compliance with Title 24 of the California Administrative Code prior to issuance of building permits.
3. Interior and exterior public area lighting shall be time-controlled and limited to the amount necessary to ensure safety of persons and property.

6.5 Noise

1. Noise standards for the specific plan area be as set forth in Table 4.

CHAPTER 7

IMPLEMENTATION

This section is intended to satisfy the requirements of Section 65451 of the California Government Code regarding a plan for implementation. Implementation measures include regulations, programs, public works projects and financing measures necessary to carry out the plan. In addition, the specific plan should include a statement on its relationship to the general plan.

7.1 CEQA Compliance

The Southwest Dinuba specific plan is a "project" that requires environmental review pursuant to the California Environmental Quality Act (CEQA). A Program EIR has been prepared for the specific plan. In the absence of substantial change, as determined by an environmental assessment, the EIR certified for this specific plan will serve as a "master" EIR for all future development.

7.2 Relationship to the General Plan

The proposed plan designation will be, "Southwest Dinuba Specific Plan." When the Dinuba General plan is not specifically superseded by this specific plan, provisions of the General Plan shall apply.

7.3 Zoning

Zoning for the plan area is shown on the specific plan map as well as discussed in Chapter 3.

7.4 Amendments to the Specific Plan

Amendments to the specific plan shall be processed in accordance with City of Dinuba standard procedures. Minor changes in the project that do not increase or decrease infrastructure capacity beyond the specified density range are only subject

Table 4

Exterior Noise Limits

(Levels Not To Be Exceeded More Than 30 Minutes In Any Hour)

Land Use	Time Period	Noise Level
Residential	10 pm - 7 am	50(dBA)
	7 am - 10 pm	60
Multiple Dwelling Residential	10 pm - 7 am	55
	7 am - 10 pm	60
Limited Commercial Some Multiple Dwellings	10 pm - 7 am	55
	7 am - 10 pm	60
Commercial	10 pm - 7 am	60
	7 am - 10 pm	65
Industrial	Any Time	70

2. Hours of construction in the plan area shall be limited from 7:00 a.m. to 7:00 p.m. during the week and 7:00 a.m. to 5:00 p.m. on Saturday unless special allowances are made by the Community Development Director. Additionally, residential-type mufflers shall be maintained on all mobile and stationary construction equipment.
3. Noise from solid waste pick-up areas, loading docks and mechanical equipment shall be mitigated by shielding these areas from adjacent residential uses. This may be accomplished by locating facilities on the opposite side of buildings from noise sensitive uses or by constructing noise barriers.

6.6 Safety and Seismic Safety

1. Prior to approval of tentative parcel or tract maps, a qualified soils engineer or engineering geologist shall prepare a detailed soils report which determines the specific characteristics and capabilities of the underlying soils and recommends appropriate measures for implementation in the design of proposed structures.
2. Proposed structures shall be constructed in accordance with the effective edition of the Uniform Building Code (UBC) for Zone II. Implementation of additional measures will be initiated, as necessary, based on the detailed soils analysis, to reduce any secondary seismic impacts to an insignificant level.

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Zoning for the plan area is shown on the specific plan map as well as discussed in Chapter 3.

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Amendments to the specific plan shall be processed in accordance with City of Dinuba standard procedures. Minor changes in the project that do not increase or decrease infrastructure capacity beyond the specified density range are only subject

to administrative review and approval by the Director of the Community Development Department.

The following changes will alter the development character and shall require an amendment to the specific plan:

1. Major changes in land use or major changes in road alignment.
2. Changes in land use types.
3. Changes in the infrastructure (roads, water and sewer systems) which have the effect of increasing or decreasing capacity beyond levels necessary to serve the project.

7.5 Financing Mechanisms

Financing mechanisms for the project include developer improvements, development fees, special district assessments or their equivalent and redevelopment financing.

Developer Improvements - The developer shall bear responsibility for development of project-related on-site and off-site improvements as described in the specific plan.

Development Fees - Developer fees shall be collected as adopted by the Dinuba City Council and required by law.

The City will initiate a fee reduction formula as an incentive to industrial development. Up to 50% of total development fees may be waived based upon an incentive point system as shown below.

Table 5

City of Dinuba Industrial Development Incentive
Fee Reduction Formula

(100 POINTS POSSIBLE)

POINT VALUE	CATEGORY AND SCORE CALCULATION
25 POINTS	JOB CREATION <ul style="list-style-type: none">- 75 TO 124 Jobs - 5 Pts awarded- 125 to 199 Jobs - 10 Pts awarded- 200 to 499 Jobs - 15 Pts awarded- over 500 Jobs - All Pts awarded
25 POINTS	LOW SYSTEM IMPACTS <ul style="list-style-type: none">- Wastewater Discharge at 75% of discharge limit - 5 Pts awarded- WW Discharge at 50% of limit - 10 Pts awarded- WW Discharge at 25% of limit - 15 Pts awarded- On-site elevated water storage tank provided - 5 Pts awarded- Flex hours or shift changes at off-peak times - 5 Pts awarded
20 POINTS	WORK FORCE UP-GRADE <ul style="list-style-type: none">- 10-19% of Jobs upgraded in 3 years - 5 Pts awarded- 20-29% of Jobs upgraded in 3 years - 10 Pts awarded- 30% or more Jobs upgraded in 3 years - All Pts Awarded
20 POINTS	ANNUAL REVENUE GENERATION - Calculated on the following: <ul style="list-style-type: none">- 20% of <u>Net</u> payroll (No benefits, deductions included)- Sales Tax- Property Tax- Redevelopment District Increment- Other <ul style="list-style-type: none">• \$175,000 - 249,000 - 5 Pts awarded• \$250,000 - 399,000 - 10 Pts awarded• More than \$400,000 - All Pts awarded
10 POINTS	QUALITY OF LIFE FACTORS (Such as attractive facilities, donations of lands and/or needed equipment to the City, provision of day care facilities, etc.) - Award of these points is not based on specific criteria.

Source: City of Dinuba

Southwest Dinuba Specific Plan

Redevelopment Financing - A portion of the plan area is now located within the redevelopment project area. Development, or tax increment, financing through the sale of bonds can be used to assist development. Every effort should be made to include the balance of the plan area (perhaps excluding residential areas) into the redevelopment project area. This will occur over time as property is annexed to the City and the requirements of state redevelopment law relative to blight and vacant land are met.

Special District or Equivalent - A benefit/assessment district or an equivalent entity shall be formed to assist in the construction and maintenance of infrastructure improvements.

This entity may be responsible for construction and maintaining:

- Interior road system
- Water supply and water system
- Sewage collection system
- Street Lighting
- Landscaping and public open space

